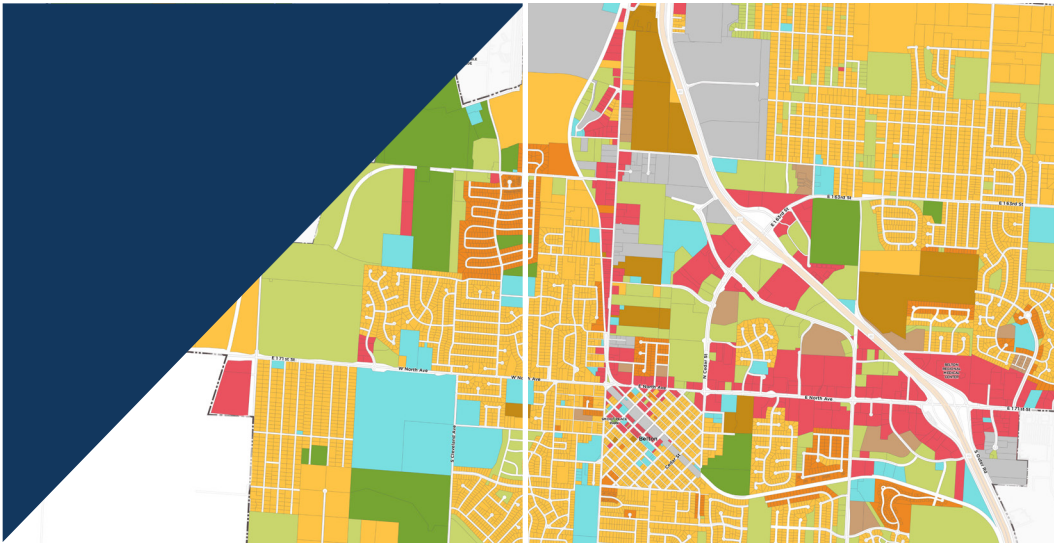


BELTON COMPREHENSIVE PLAN

existing conditions summary report



INTRODUCTION

Overview

The intent of this report is to summarize all the findings from Phase 1 of the planning process. Within this phase, thorough data analyses and projections were completed to better understand the current conditions of Belton and what the community may look like by 2050.

Topics ranging from current household composition to locations of the 100-year floodplain are discussed at length within this report and will further supplement the Belton 2050 Comprehensive Plan.



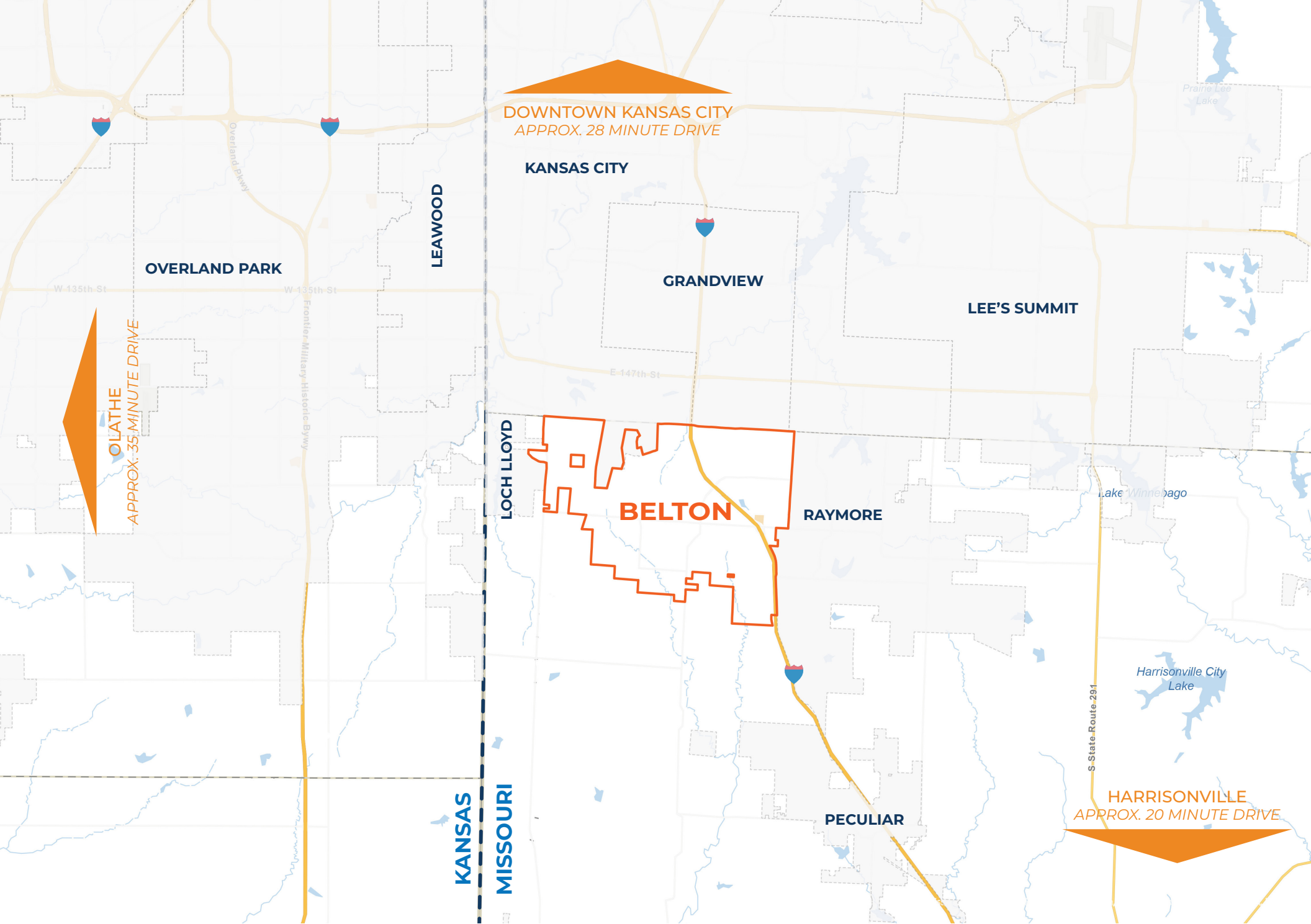


FIGURE 1.1 // REGIONAL CONTEXT MAP OF BELTON

DEMOGRAPHIC OVERVIEW

Population Trends

Historic population trends prior to the 1990s demonstrate rapid population growth from 1960-1970 due to the Richards-Gebaur Air Force Base opening. The population soared from an estimated 4,897 residents in 1960 to approximately 12,270 residents by 1970. While the Air Force Base closed in the mid-1990s, population has steadily increased in Belton.

Figure 1.2 illustrates population trends for Belton from 1990 to 2022. The estimated population in 1990 was 18,154 residents. It has since experienced an estimated 33% increase to 25,420 residents in 2022.

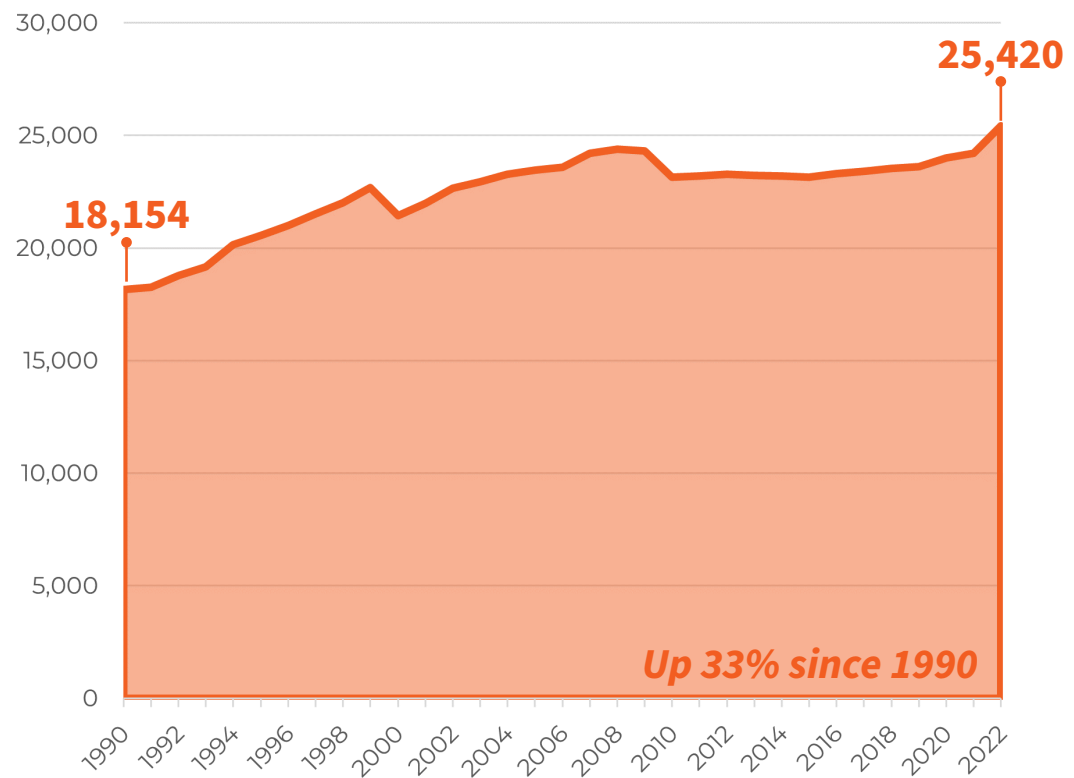


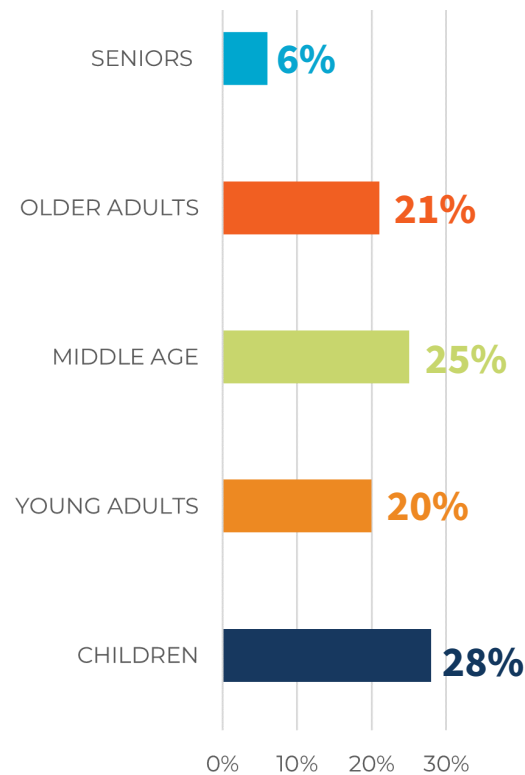
FIGURE 1.2 // BELTON POPULATION ESTIMATES FROM 1990-2022
SOURCE: U.S. CENSUS BUREAU ACS 5-YR 2021 ESTIMATES



Age Composition

To better understand the make-up of a community, an age composition analysis can be completed to see the share of males vs females by age cohorts. Belton has a median age of 35.4 years, which is slightly younger than that of Cass County and other surrounding Kansas City Metro communities.

Children, ages 20 and under, assume the highest age cohort in Belton. Middle Age (ages 35-54) residents are the second highest and can assist in drawing the conclusion that Belton may be a family-centric community, supporting the high percentage of children.



35.4 years
median age in Belton.

Belton is slightly younger than Cass County, the KC Metro, and some surrounding Metro neighbors.

FIGURE 1.3 // AGE COMPOSITION IN BELTON
SOURCE: U.S. CENSUS BUREAU ACS 5-YR 2021 ESTIMATES

HOUSEHOLD COMPOSITION IN BELTON



there are an estimated

9,171

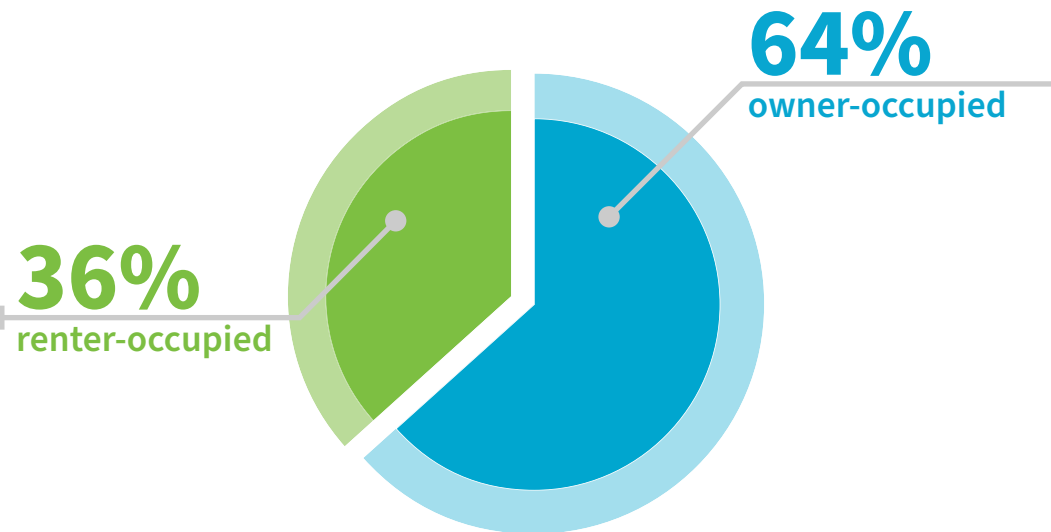
total households in Belton today.

30.3%

households have one or more people under 18 years old.



SOURCE: U.S. CENSUS BUREAU 2020 DECENNIAL CENSUS



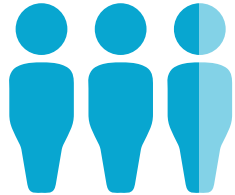
There are an estimated 9,363 occupied residential units in Belton today. Occupancy is skewed toward more ownership, which is typical for a suburban community, like Belton.

**BELTON AVERAGE
HOUSEHOLD SIZE**



2.5 PERSONS

**HOMEOWNER AVERAGE
HOUSEHOLD SIZE**



2.6 PERSONS

**RENTER AVERAGE
HOUSEHOLD SIZE**



2.5 PERSONS

**BELTON MEDIAN
HOUSEHOLD INCOME**



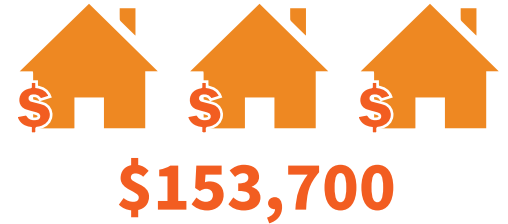
**HOMEOWNER MEDIAN
HOUSEHOLD INCOME**



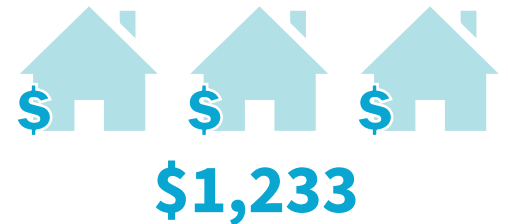
**RENTER MEDIAN
HOUSEHOLD INCOME**



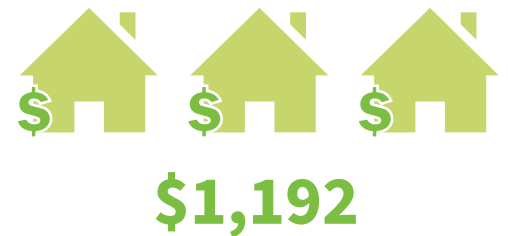
**BELTON MEDIAN
HOME VALUE**



**HOMEOWNER MEDIAN
MORTGAGE COST**



**RENTER MEDIAN
GROSS RENT**



SOURCE: U.S. CENSUS BUREAU ACS 5-YR 2021 ESTIMATES

PHYSICAL PROFILE



Overview

In order to better plan for the future of the community, a thorough analysis of the existing conditions and operations was completed. This section summarizes the existing land uses, residential data, natural features, and infrastructure components that impact the community today.



Existing Land Uses

The existing land use analysis helps to understand the current land development patterns and operations in Belton today. Everything from agriculture land to large business parks are defined and accounted for.

This map differs from a zoning map, as it illustrates the current function, not its legal zoning classification. These two maps combined help guide decisions for ideal land use patterns within the Future Land Use Plan.



Residential Parcel Data

Residential developments come in a variety of sizes, densities, and other defining characteristics. This section aims to dive into more of the parcel-specific functions to see where there might be gaps in housing type and price.

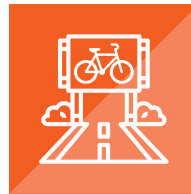
This section is supplemented by the U.S. Census data discussed earlier in this report.



Existing Natural Features

Natural features encompass floodplains, streams, and waterbodies. These features often serve as aesthetic components for the community and are well-complimented by recreational amenities.

These features are critical to inventory to proactively preserve and buffer from future development. This factor is discussed in greater detail within the comprehensive plan.



Existing Parks + Trails

Parks and trails provide much needed amenities and quality of life components to the community. Each park and trail is inventoried in Belton to evaluate park types, connections, and existing levels of service.



Existing Infrastructure

Transportation, water, sanitary sewer, and stormwater infrastructure contribute to a well-functioning community. These components are discussed at a high-level to identify current operations and potential gaps in services that may influence development potential in the future.

EXISTING LAND USES

Existing Land Uses in Belton Today

The map shown in Figure 1.5 presents the existing land uses identified in Belton today. Single-family homes (34.0%) are the predominant land use type, followed by Ag + Open Space (23.4%) and Commercial (6.6%) uses.

The map follows general land use development patterns:

- Commercial and industrial uses along major roadways (Interstate 49, 58 Highway, and N Scott).
- Decreasing intensities of uses from major roadways.
- Mix of single-family homes, duplex/triplex/quadplex, and multi-family residential uses.
- Parks integrated throughout the community.

LAND USE FUN FACT:

Of Belton's 467 acres of Public/Semi-Public land, 16% of it is occupied by churches.

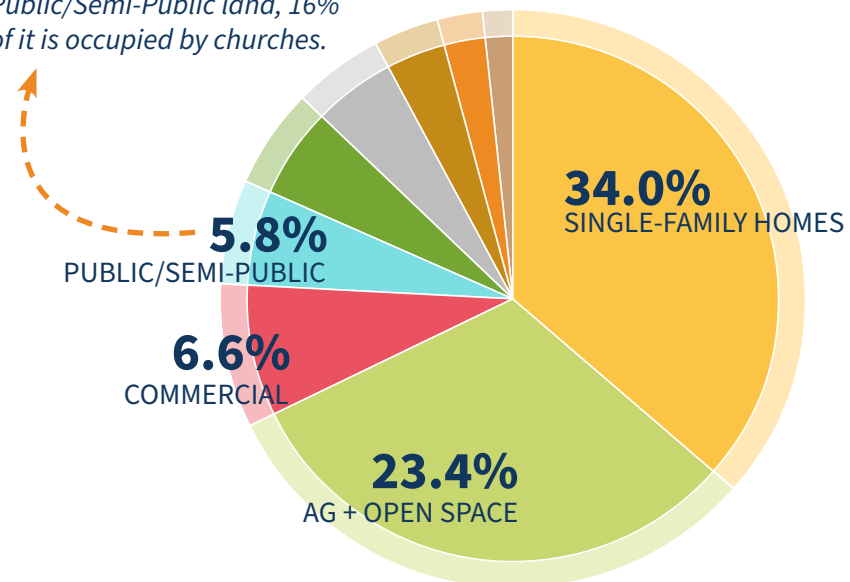


FIGURE 1.4 // EXISTING LAND USE TOP FOUR BREAKDOWNS

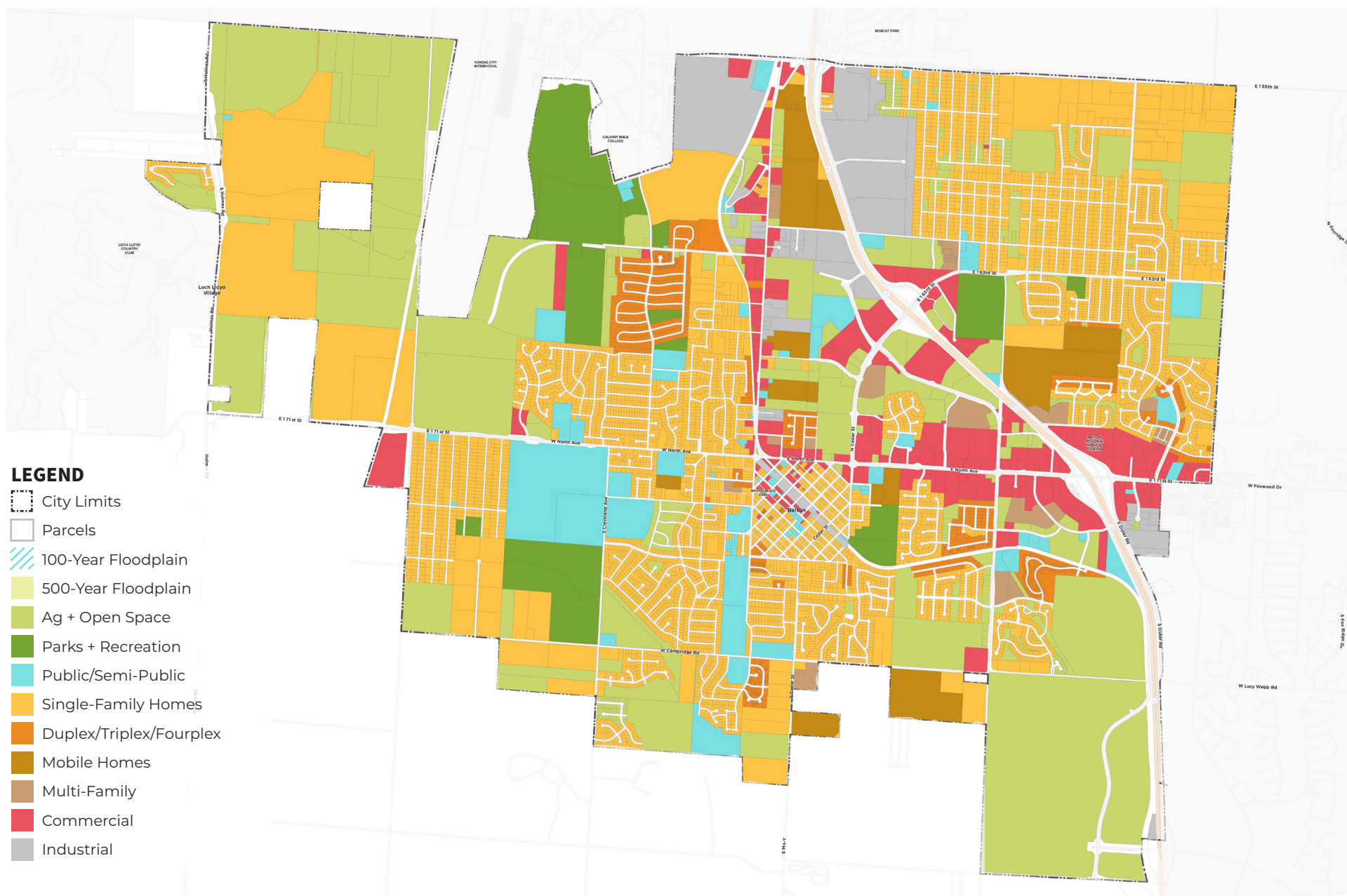


FIGURE 1.5 // EXISTING LAND USE MAP

EXISTING RESIDENTIAL LAND USES



Existing Residential Patterns

Existing residential land uses in Belton are primarily comprised of single-family detached units. However, Belton does have a healthy presence of medium- and high-density housing types in the forms of duplexes, triplexes, fourplexes, manufactured home parks, and apartments. This is likely due in part to the Richards-Gebaur Air Force Base having been located in Belton and needing to house a large group of residents with denser development patterns. The Base has since closed, but the housing remains available within the community.

These current shares of residential land use types will be taken into consideration for the Future Land Use Plan and the estimated target mix for housing composition within the Plan. The target mix serves as a secondary guide to ensure the proposed Future Land Use Plan aligns with population projections, housing demand estimates, and builds in a healthy mix of housing types for ample housing mobility within the community.

LAND USE TYPE	ACRES	SHARE
Single-Family Homes	2,981.1	81.4%
Duplex/Triplex/Fourplex	247.4	6.8%
Mobile Homes	284.0	7.8%
Multi-Family Residential	118.1	3.2%
Senior or Nursing Facilities	33.3	0.9%
TOTAL RESIDENTIAL LAND	3,663.9	100.0%

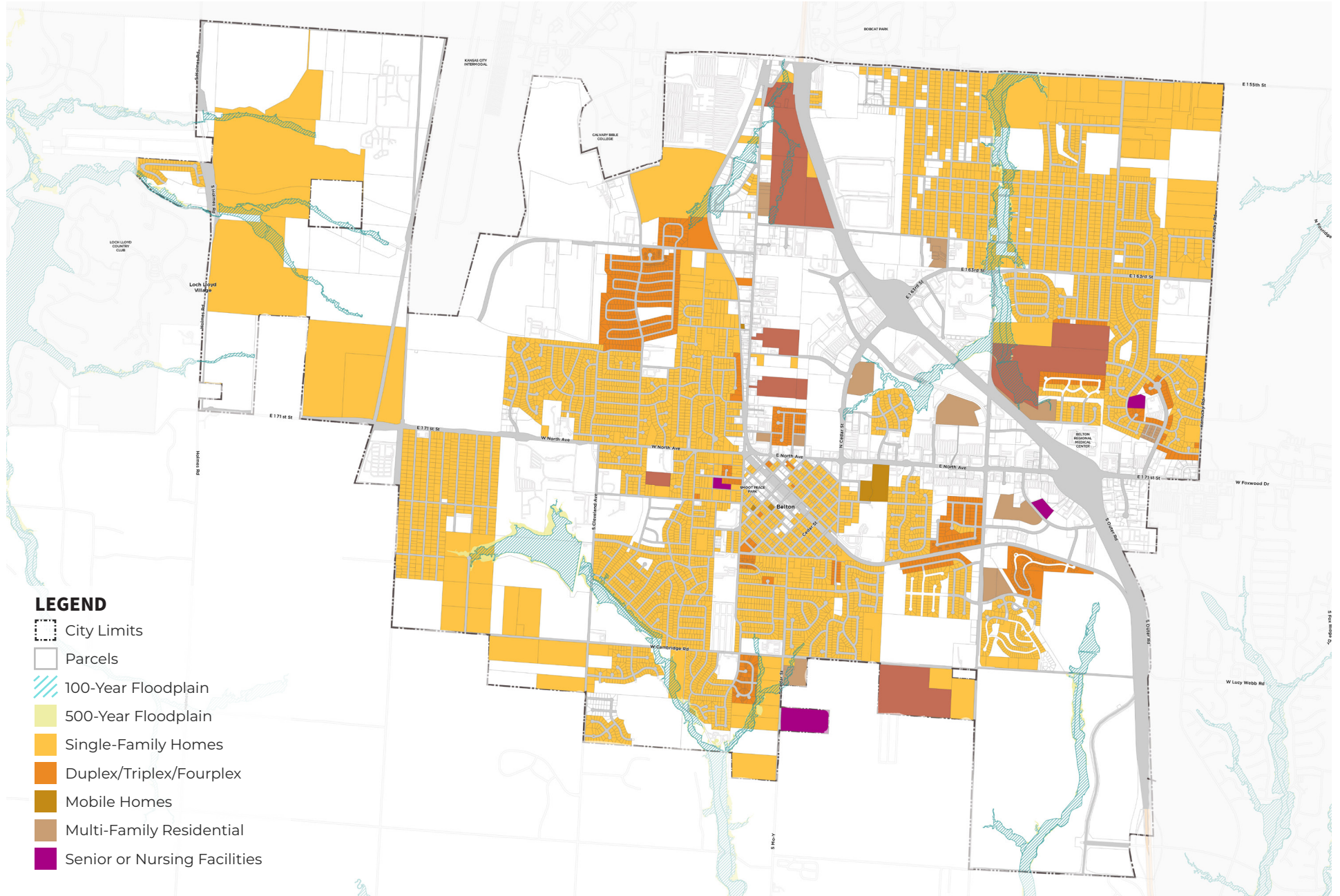


FIGURE 1.6 // EXISTING RESIDENTIAL LAND USE MAP

EXISTING RESIDENTIAL TOTAL VALUES

Residential Total Values in Belton

The map in Figure 1.7 illustrates the spread of residential total values in Belton. Total values account for both the land and housing value for each parcel. The average total value for single-family and duplex/triplex/fourplex homes is estimated to be \$148,679 according to Cass County parcel data.

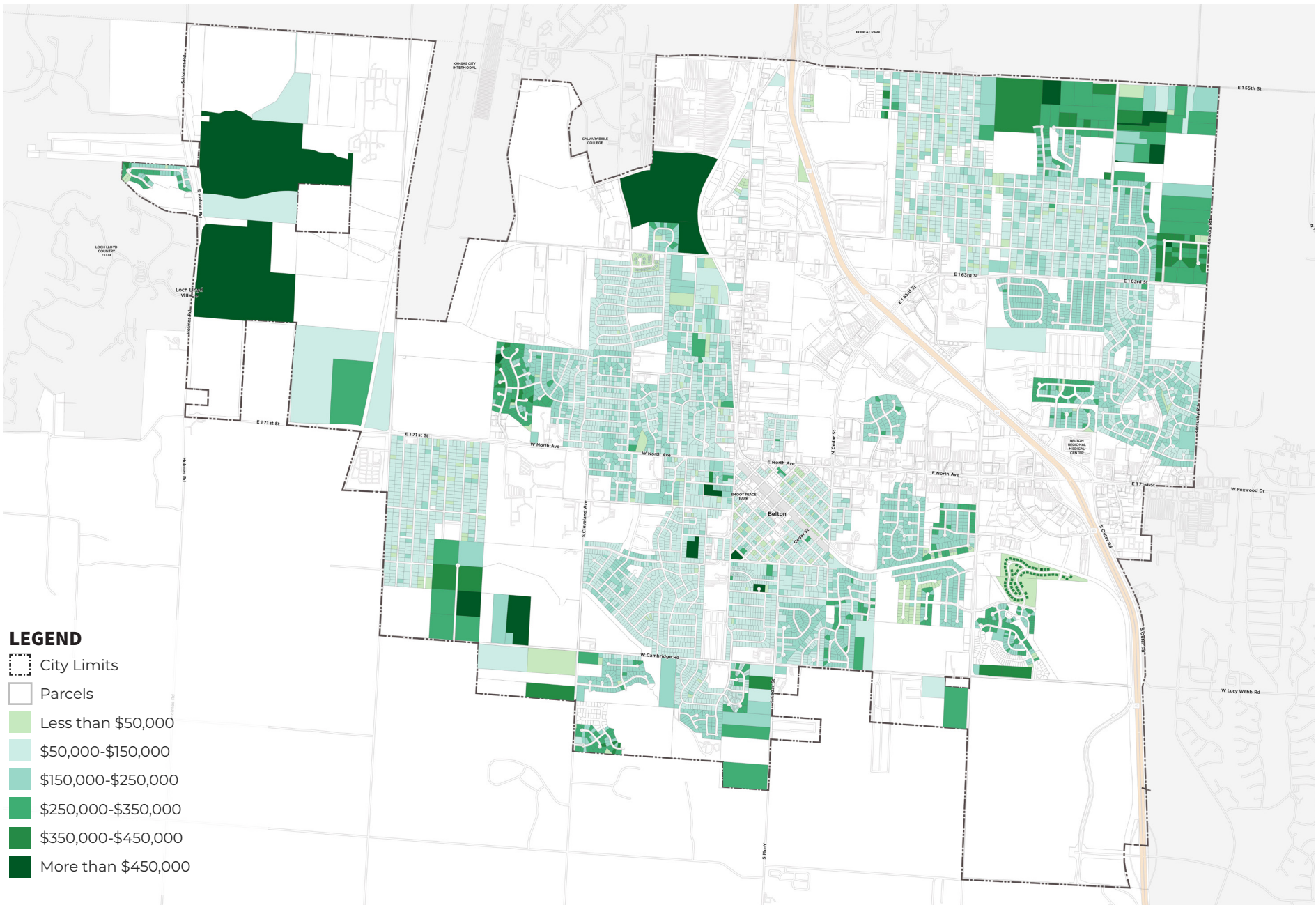
The map highlights a few key trends to take note of:

- Homes located in Old Towne Belton have lower estimated total values.
- Values typically follow development patterns, with newer homes having generally higher total values.

Total values account for both the land and structure value.

These estimates are completed by the County Assessor.

*Average Total Value of Single-Family and Duplex/Triplex/Fourplex Homes in Belton: **\$148,679***



LEGEND

- City Limits
- Parcels
- Less than \$50,000
- \$50,000-\$150,000
- \$150,000-\$250,000
- \$250,000-\$350,000
- \$350,000-\$450,000
- More than \$450,000

FIGURE 1.7 // RESIDENTIAL TOTAL VALUE MAP

EXISTING NATURAL FEATURES

Existing Waterbodies + Streams

Streams and waterbodies help to establish the natural scenery of any given area. The primary waterbody in Belton today is Cleveland Lake located in the southwest quadrant of the city. Cleveland Lake was constructed in an effort to mitigate flooding impacts to surrounding roadways and homes.

Type 2 intermittent streams are the primary stream type in Belton. Intermittent streams are those that have flows during the wet seasons of the year. These stream corridors should be buffered from adjacent development to ensure optimal flow patterns.

Impacts of the Floodplain

The floodplain is a natural resource with classifications for 100-year and 500-year floodplains. The floodplain, shown in Figure 1.8, typically follows the stream paths shown in the blue flow lines. Cleveland Lake Regional Park, the Graham-Effertz subarea, and northeast quadrant of the city have fairly high concentrations of the 100-year floodplain impacting the adjacent land operations. It is recommended that these areas be left undeveloped and preserved as a natural resource.

The City of Belton has a robust set of stream buffer requirements and corresponding design standards adopted as part of the Unified Development Code to preserve the land within or adjacent to the floodplain and/or 100-year storm event. The code outlines what qualifies under the requirements and design procedures that need to take place to stay compliant within the design process. Some highlights from the design standards are outlined in Figure 1.8.

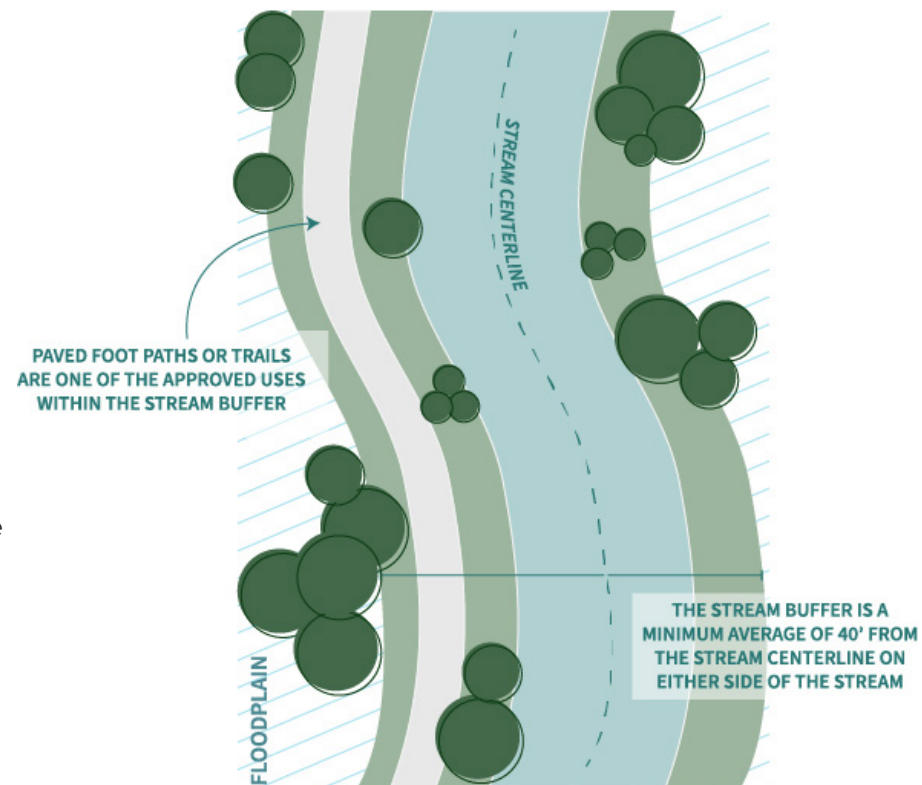


FIGURE 1.8 // STREAM BUFFER EXAMPLE

EXISTING PARKS + TRAILS SYSTEMS

Existing Parks

There are twelve park facilities located in Belton today that provide a variety of services and amenities to residents and visitors. The typical park classification types are: regional park, community park, neighborhood park, pocket park, and special use parks.

Pocket parks are quite small in size and provide limited amenities. Neighborhood parks are larger than pocket parks but smaller than community parks and provide immediate access to park space for residential areas. Community parks are much larger, typically greater than 5 acres, and have more programmed space and activities. Regional parks provide services to the community and beyond and likely serve as destinations with longer lengths of stay. Special use parks often serve one unique purpose, such as a dog park or skate park and vary in size.

The current Belton park acres and types are broken down in detail in Table 1.1.

Existing Trails

Trails in Belton are primarily situated on-site at existing parks. Most trails in Belton have a hard surface and are classified as shared use paths. Shared use paths serve more as shared facilities for pedestrians and bicyclists.

A Future Trails Plan will be discussed in greater length within the body of the comprehensive plan to identify recommended routes and destinations for areas in Belton via trail routes.

Level of Service Analysis

To better understand how well the community is currently being served by parkland and trails, a level of service analysis (LOS) was completed. This type of analysis can take the form of park acres per thousand residents, trail miles per two-thousand residents, a walk-time analysis, and park service areas.

Park Acres Per 1,000 Residents

The National Recreation and Park Association (NRPA) sets a national standard for 10.5 total park acres per 1,000 residents. Today, Belton's LOS is slightly higher than the recommended standard. Strategies and recommendations for improving this LOS are found throughout this chapter.

The current trails LOS is estimated to be .53 miles per two-thousand residents. This is also lower than the recommended standard and should be considered in tandem with road and park expansions to improve trail mileage and routes in and around Belton.

PARK NAME	ACRES	PARK TYPE
Cimarron Trails Park	5.6	Neighborhood
Cimarron Trails Ball Field	5.2	Neighborhood
Cleveland Lake	50.8	Regional
Country View/West Belton Park	6.7	Neighborhood
Dryden Family Arboretum	69.0	Regional
Markey Park	56.8	Community
Memorial Park	33.5	Community
Military Park	6.5	Neighborhood
Smoot Peace Park	0.5	Pocket
Somerset Park	5.3	Neighborhood
Wallace Park	39.7	Regional
Westover Soccer Field	7.2	Neighborhood
TOTAL PARK ACRES	287.0	
EXISTING POPULATION	25,420	
RECOMMENDED LEVEL OF SERVICE	10.5/1,000 RESIDENTS	
CURRENT LEVEL OF SERVICE	11.3/1,000 RESIDENTS	

TABLE 1.1 // PARKS LEVEL OF SERVICE ANALYSIS

TRAIL NAME	MILES
Somerset Park Trail	0.1
Turner Road Path	0.2
Belton Nexus Trail	0.5
Markey Park Trail	0.7
Markey Parkway Path	0.8
Memorial Park Trail	1.0
Cleveland Lake Regional Park	1.1
Cedar/163rd Trail Path	1.1
Wallace Park Trail	1.2
TOTAL TRAIL MILES	6.7
EXISTING POPULATION	25,420
RECOMMENDED LEVEL OF SERVICE	1 mi/2,000 Residents
LEVEL OF SERVICE	0.53 mi/2,000 residents

TABLE 1.2 // TRAILS LEVEL OF SERVICE ANALYSIS

LEVEL OF SERVICE ANALYSIS CONTINUED

Walk-Time Analysis

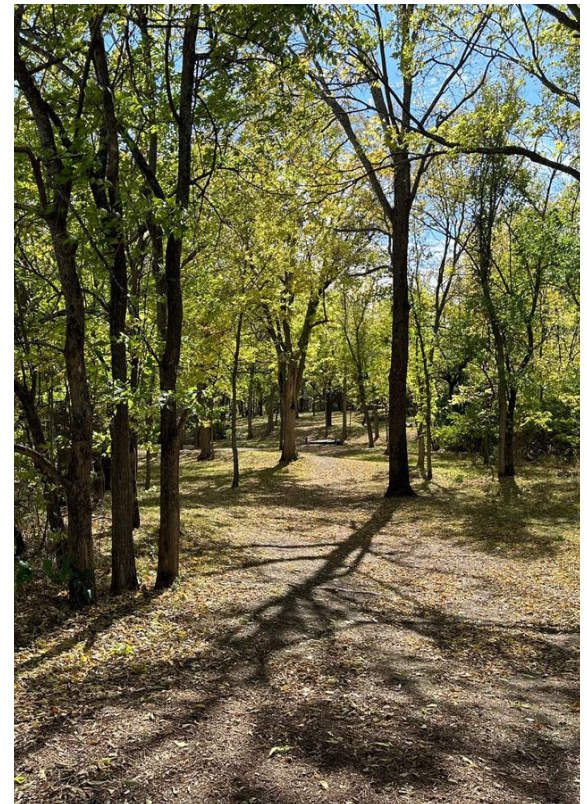
A walk-time analysis was completed utilizing mapping software to better estimate residents within 5-, 10-, and 15-minute walk times to existing parks. Figure 1.11 highlights these walk-times in graduated rings. Approximately 65% of all residential parcels are within a 15-minute walk to a park in Belton today.

Service Area Analysis

A service area analysis assigns varying distances by park type. Neighborhood parks receive 1/4 to 1/2 mile service distance. Community parks serve a wider area and receive a 1/2 to 1 mile service distance. Special use parks do not have an assigned buffer distance due to their unique amenities.

Figure 1.11 shows the service distance areas overlaid by the walk-time analysis. The breakdowns by service area are as follows:

- 1/4-mile: 1,171/7,699 parcels (15%)
- 1/2-mile: 4,474/7,699 parcels (58%)
- 1-mile: 6,846/7,699 parcels (89%)



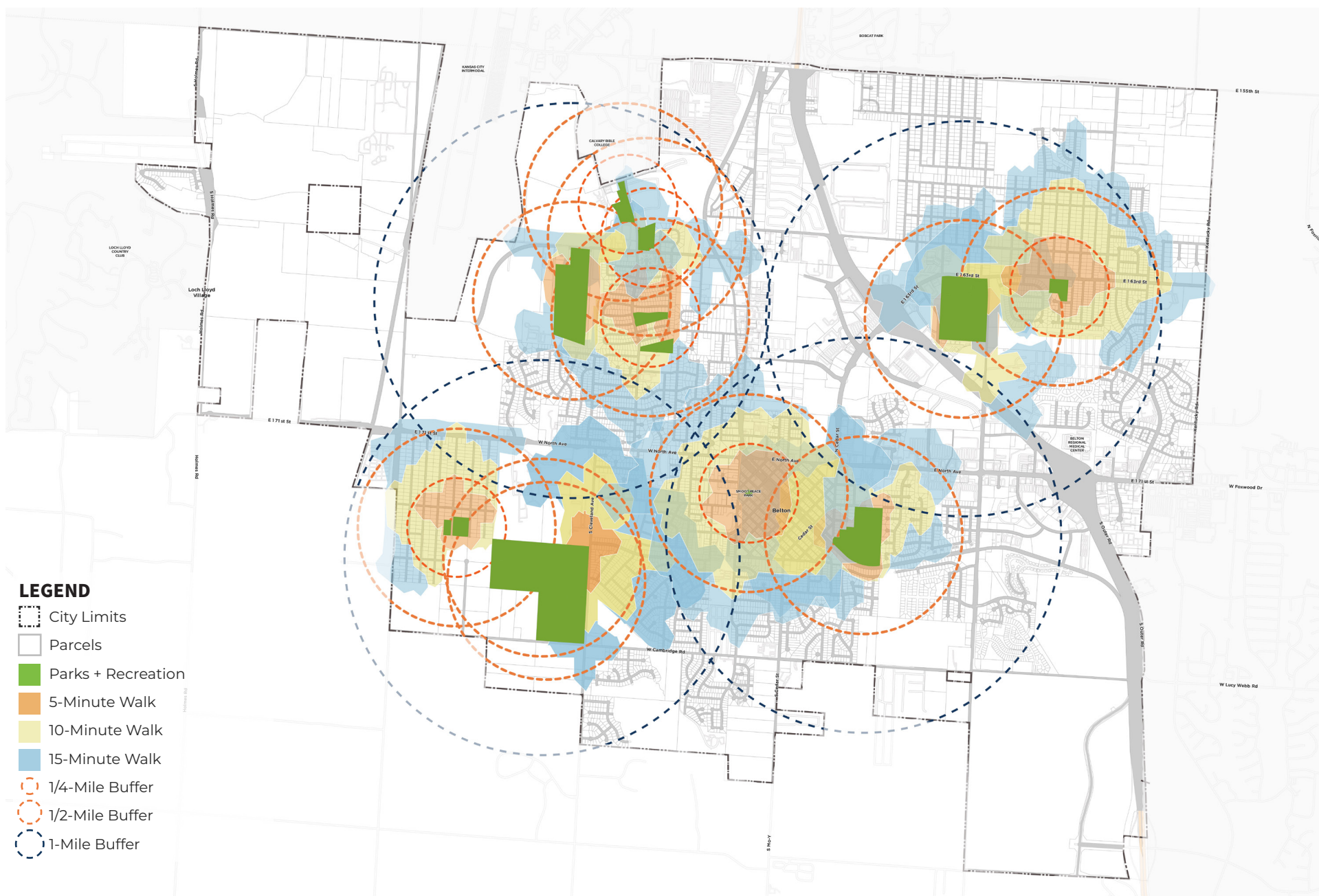
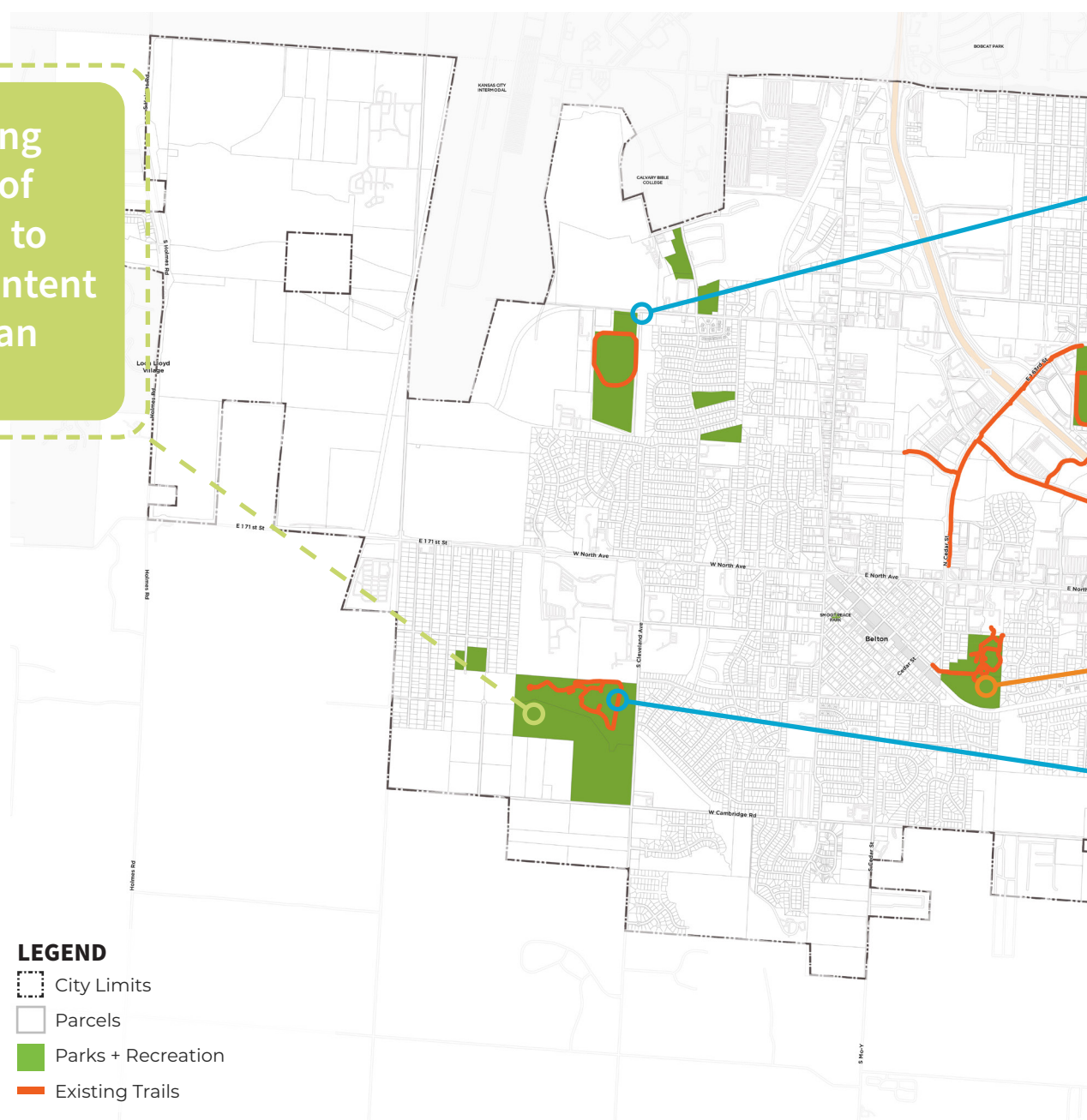


FIGURE 1.10 // PARK SERVICE AREA AND WALK-TIME MAP

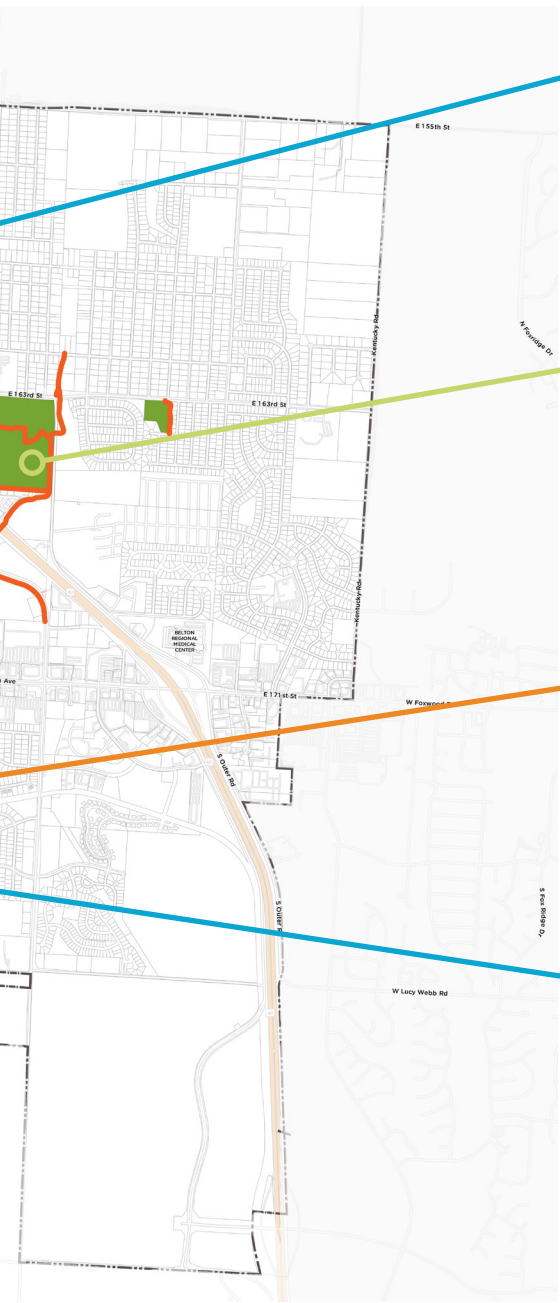
During the planning process, 69 acres of land was donated to the City with the intent it would serve as an arboretum.

Belton's level of service is lower than recommended today and should be a priority for expansion. These existing trails provide opportunities for greater community connections and mobility for residents.



- LEGEND**
- City Limits
 - Parcels
 - Parks + Recreation
 - Existing Trails

FIGURE 1.11 // EXISTING PARKS AND TRAILS MAP



Markey Park is a 45-acre park that has five ball fields, a 5/8-mile walking trail, and playground.

Wallace Park is a 38-acre park home to the High Blue Wellness Center, skate park, sports courts and fields, and a 7/8-mile walking trail.

Memorial Park features a water park, Memorial Station event space, playground, shelters, sports courts and fields, and more.

Cleveland Lake features a lake for paddle boarding, boating, and fishing, a 1.2-mile walking path, disc golf course, and scenic views.

TRANSPORTATION INFRASTRUCTURE

Current Services

Roadways

Belton is served by a network of roadways with the north-south I-49 and the east-west Missouri State Highway 58 (North Avenue) corridors representing the spines of the transportation network. While I-49 is an asset to spur economic development and residential growth, the interstate can also be a barrier to east-west connectivity as it bisects the community. Access to I-49, which serves over 40,000 vehicles per day, is provided through four interchanges: 155th Street, 163rd Street, Missouri State Highway 58 (North Avenue), and North Cass Parkway. As the primary gateways into Belton, these interchanges experience the highest traffic volumes on roadway network.

Three levels of government have jurisdiction over roadways in Belton: City, State, and Federal. The federal government oversees the interstate system including I-49. State roadways include Route Y (Cedar Street) and Highway 58 (North Avenue). Within the city's jurisdiction are both public and private roadways. Most private roadways in the city serve individual properties or developments.

63.3



Belton's average Pavement Conditions Index Rating.

The Pavement Conditions Index (PCI) rating system examines the health of roads. Roads are assessed on a scale of 0 (poor) to 100 (newly paved road), with factors such as age, climate, and traffic volumes impacting its rating.

Generally, ratings of 0-50 indicate a need for future reconstruction; 51-70 a need for rehabilitation; and 71-100 a need for minor improvements (crack sealing or seal coating).

TRANSPORTATION INFRASTRUCTURE CONT'D

Bicycle + Pedestrian Routes

The active transportation network in Belton generally includes sidewalks, trails, and shared-use paths. These bicycle and pedestrian facilities provide important, non-vehicular ways for residents to travel around the community while also increasing quality of life, physical health, and social interaction in Belton. More than 60 miles of sidewalks connect neighborhoods and activity centers throughout the city, shown in the Active Transportation Map. Existing trails including the Cleveland Regional Lake Trail, Markey Park Trail, Somerset Park Trail, Wallace Park Trail, Belton Nexus Trail, and Memorial Park Trail. Shared-use paths along Cedar Street, Turner Road, and Markey Parkway provide off-street accommodations on some of the city's busiest roadways.

Transit

The Kansas City Area Transportation Authority (KCATA) is the primary public transportation provider in the Kansas City region via the RideKC bus system. The RideKC network provides a commuter bus route from downtown Kansas City to Grandview, but services do not currently extend to Belton. Belton residents who wish to use transit can travel to Grandview and utilize the free Park & Ride service located at Second Missionary Baptist Church. For eligible residents (seniors and people living with a disability) OATS Transit, which stands for Operating Above the Standard, offers transportation for trips *within* Belton on the first, second, third, and fourth Wednesday of each month. On the fourth Wednesday, OATS provides medical transportation from Belton to Grandview, Harrisonville, and Raymore.



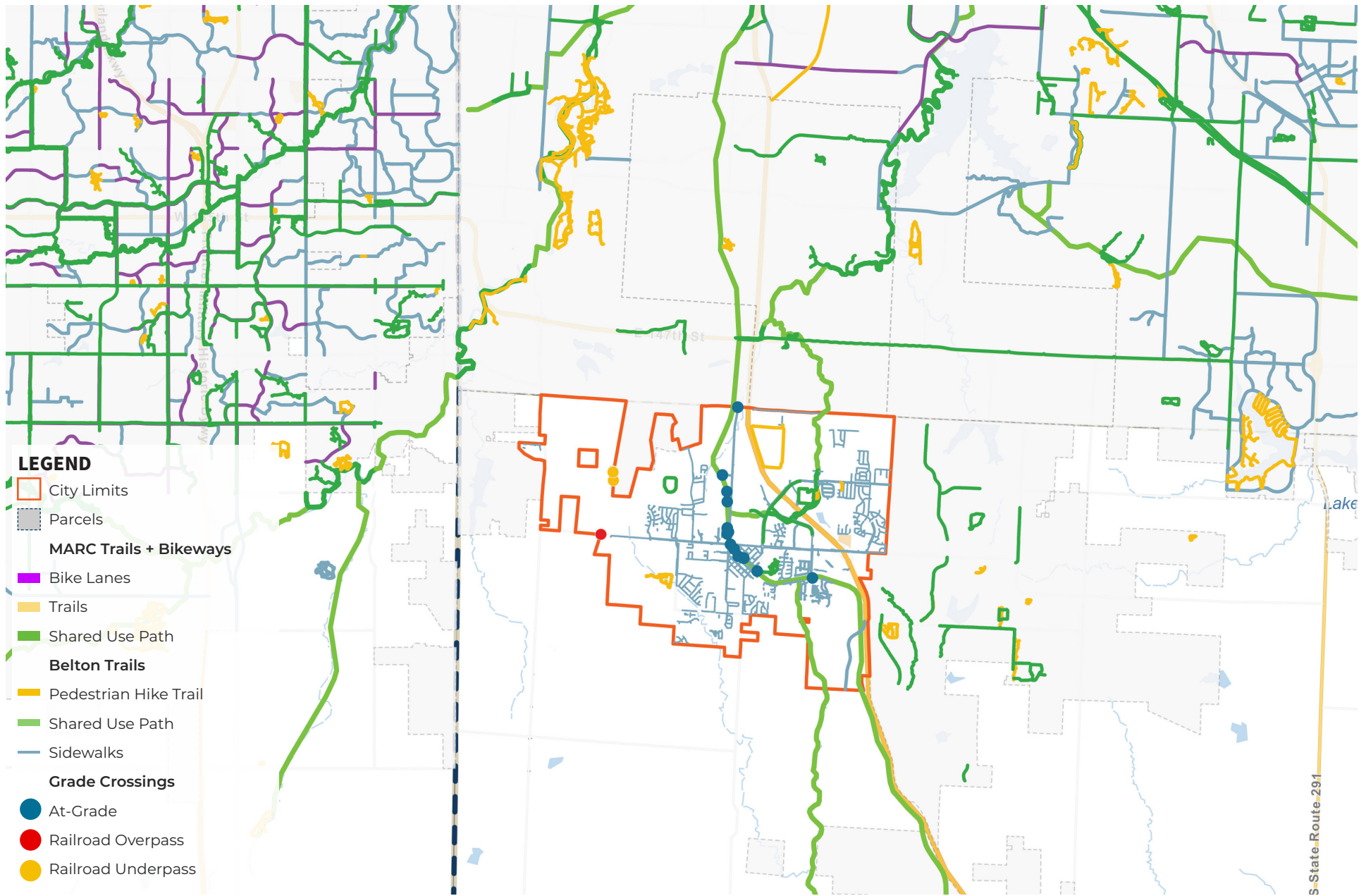


FIGURE 1.13 // EXISTING REGIONAL TRAILS + PATHWAY CONNECTIONS

TRANSPORTATION INFRASTRUCTURE CONT'D

Current Services

Railroads

While railroads are an important national asset for freight movement and have historically spurred growth in small cities, rail corridors can also act as a physical barrier within communities. In early 2023, the Canadian Pacific Railroad acquired Kansas City Southern railroads, now known as the Canadian Pacific Kansas City (CPKC) Railroad. Two CPKC lines pass through city limits: the Pittsburg Subdivision, west of downtown, and the Grandview Industrial Lead which passes through downtown and terminates near the intersection of Cambridge Road and Peculiar Drive. Approximately 16 trains currently travel the Pittsburg rail corridor each day, but train volumes are anticipated to grow due to the merger. The industrial lead passing through downtown is underutilized by local businesses, leaving the tracks dormant except for the Belton, Grandview, and Kansas City Railroad Co. short line passenger railroad and demonstration museum which operates excursions on weekends.

There are 19 highway-rail crossings in Belton, most of which are at-grade. According to the Federal Railroad Administration's crossing inventory, there are three grade-separated railroad crossings within city limits. The CPKC Pittsburg Subdivision crosses under 58 Highway and over Prospect Avenue and the private entrance to Carril Los Amigos. Throughout Belton, crossing conditions vary with some crossings featuring sidewalk connectivity and fresh pavement while others are in a state of disrepair.

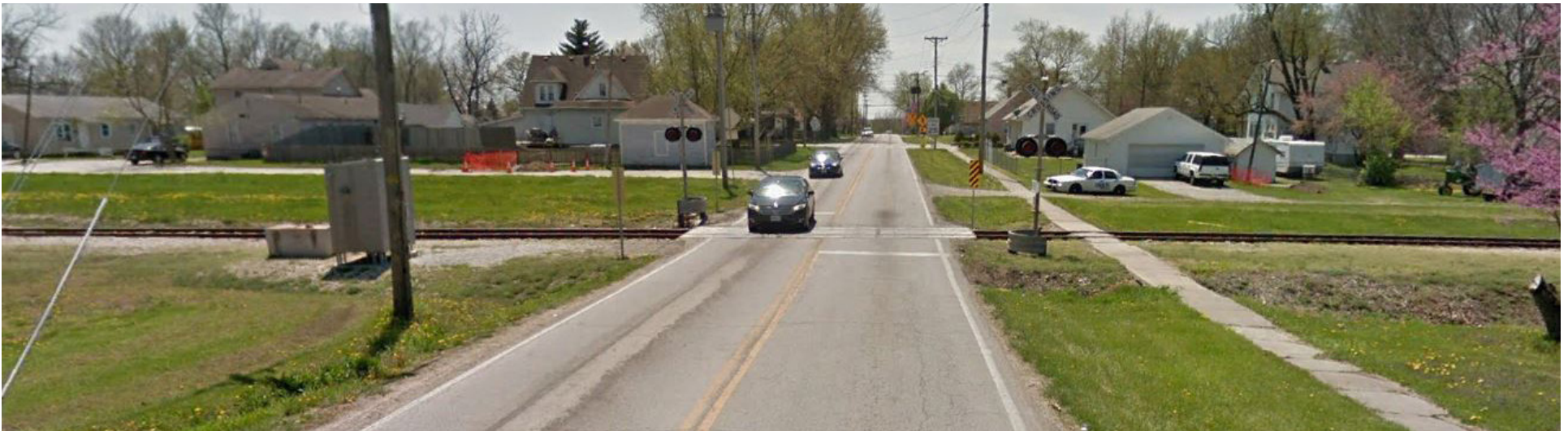


FIGURE 1.14 // EXAMPLE AT-GRADE CROSSING IN BELTON

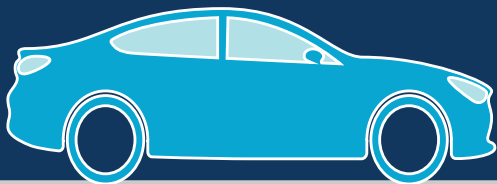
EXISTING INFRASTRUCTURE FAST FACTS



Belton is actively working on seven Priority 1 storm sewer improvement projects. There are 60 miles of storm sewer lines serving the community.

80% 

of Belton's water comes from the Missouri River.



there are an estimated

263

city-owned lane miles in Belton.

Treated water from the wastewater treatment plant is released into East Creek. The treatment plant serves 22 sq.mi. with 8 pump stations and 136 miles of sewer lines.



WATER INFRASTRUCTURE

Current Services

Figure 1.15 highlights the current water services provided to the City of Belton. The Water Services Division of Public Works operates and maintains the city's water reservoirs, pump stations, and water mains to meet Belton's water demands.

The city's water is purchased from the Kansas City Water Department (KC Water). KC Water obtains its water from a combination of ground and surface water sources. Approximately 80% of the water comes from the Missouri River, while 20% is sourced from the Missouri River aquifer.

Water treatment is a four-step process that consists of sedimentation, softening, stabilization, and filtration. Belton currently has nearly 150 miles of water lines, as displayed in the Water Infrastructure Map. These lines consist of pipes of various sizes and materials that deliver water to homes, businesses, and other facilities throughout Belton. The largest pipes are located along central corridors where residential development and commercial demands are the highest. Smaller pipes distribute water to neighborhoods and less demanding areas of the city. Valves, pumps, and pressure regulators are installed within the distribution network to control water flow and maintain adequate pressure.



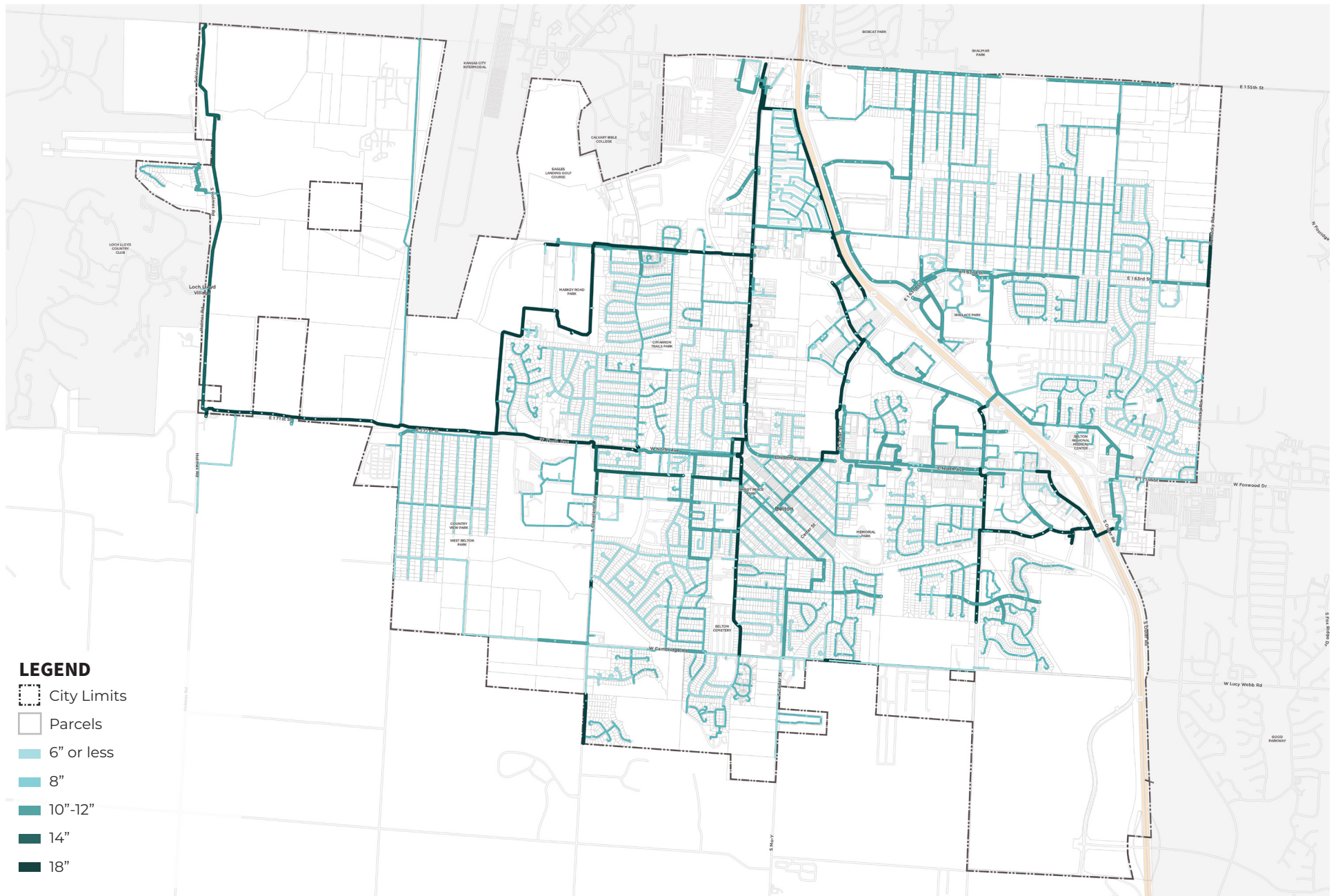


FIGURE 1.15 // EXISTING WATER INFRASTRUCTURE MAP

STORM SEWER INFRASTRUCTURE

Current Services

Storm sewers are an integral part of urban infrastructure – helping to prevent flooding, protect property, and maintain the safety and functionality of roadways during rain events. The City of Belton, Missouri is an operator of a small municipal separated storm sewer system (MS4). As part of the national Pollutant Discharge Elimination System (NPDES) Phase II requirements, the City is required to develop, implement, and enforce a stormwater management program designed to reduce the discharge of pollutants from their MS4 to the “maximum extent practicable” to protect water quality requirements of the Clean Water Act.

The City of Belton’s Stormwater Master Plan was completed in 2012 and identified 30 stormwater projects throughout the City. Of these 30 projects, seven were categorized as Priority 1 projects. These types of projects are characterized by frequency and severity of home flooding, street flooding, erosion, and regional benefit. At the time of this plan, two of the projects have been completed while the remaining five are to be completed with the Proposition S stormwater general obligation bonds. Belton currently has nearly 60 miles of storm sewer lines, shown in the Storm Sewer Infrastructure Map in Figure 1.16.





- LEGEND**
- City Limits
 - Parcels
 - 12" or less
 - 15"
 - 18"
 - 20"-24"
 - 30"
 - 36"
 - 60" or more

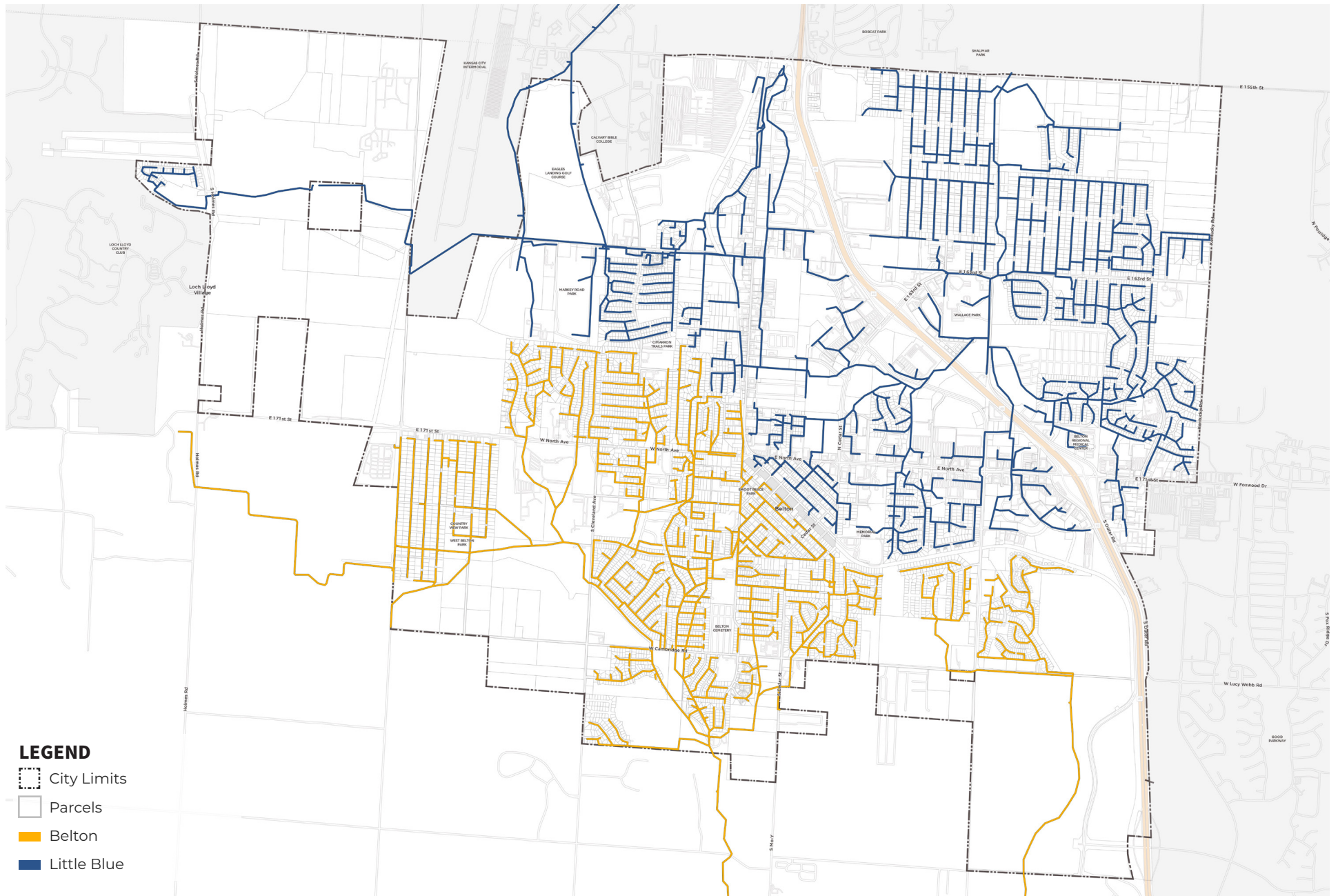
FIGURE 1.16 // EXISTING STORM SEWER MAP

SANITARY SEWER INFRASTRUCTURE

Current Services

The Water Services Division of Public Works is responsible for the operation and maintenance of the wastewater collection system and treatment facility in Belton. The treatment facility was designed to treat an average flow of 2.26 million gallons and a peak flow of 7.63 million gallons daily. After reducing and removing sludge, the treated wastewater is released into East Creek. The City's sanitary sewer system covers approximately 22 square miles, consisting of eight pumping stations and more than 136 miles of piping. The system is divided into two main drainage basins. Approximately half of Belton's wastewater flows to the Little Blue Valley Sewer District, while the other half flows to Belton's wastewater treatment facility, as shown in the Sanitary Sewer Infrastructure Map in Figure 1.17.






- LEGEND**
-  City Limits
 -  Parcels
 -  Belton
 -  Little Blue

FIGURE 1.17 // EXISTING SANITARY SEWER MAP

